



The Town of Coventry Special Planned Overlay District:

Route 3/Sandy Bottom Road Special Management District

General Business Design Guidelines

Prepared by:
The Coventry Planning
Department

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1.0 INTRODUCTION

- Studies have shown that aesthetic controls are vital in planning because the built environment is a central part of human existence. The environment in which we live can induce a sense of identity, territoriality, and belonging. The U.S. Supreme Court has gone one step further to uphold the authority of a legislative body (i.e. Town Council) to enact laws which impose aesthetic requirements in a given area (Berman v. Parker, 348 U.S. 26).
- As noted in the Town of Coventry Comprehensive Community Plan, Amended June 19, 2000, *“Tiogue Avenue / Nooseneck Hill Road (Route 3) constitutes the primary commercial service spine in the Town of Coventry running from east to west in the southern portion of the community. The corridor has evolved from that of a series of isolated “strip” commercial areas in the 1960s and 1970s into a nearly linear pattern of commercial, retail and service development that today has serious image and functional problems.”*
- Land-Use 2025, the forward-thinking land-use plan implemented by the State of Rhode Island stresses the need for a shared vision for protecting Rhode Island’s characteristic urban-rural balance with desired development patterns focused on compact, mixed-use and accessible centers highlighted by greenspace. It also advances the necessity for state and local efforts to accomplish this vision of better utilizing our existing assets, safeguarding our environment and intensifying our economic aptitude.

Route 3 Today



1.0 INTRODUCTION

- In order to improve public safety and enhance traffic handling capacity, the Rhode Island Department of Transportation (RIDOT) will soon go out to bid on a 5-year capital improvement project involving street functionality and streetscape improvements along Route 3. Appendix A outlines this plan in greater depth. The project, along with subsequent reconstruction activities, offers the Town of Coventry an opportunity to plan for a comprehensive revitalization of a significant commercial corridor to meet contemporary functional and design needs.
- In an effort to promote local revitalization efforts of both **Route 3** and **Sandy Bottom Road**, the Coventry Department of Planning and Development has created the design guidelines herein.
- The guidelines are intended to convey desirable elements. They are recommendations and not requirements, unless public financing is involved in the construction or rehabilitation of the building. Standards identified are enforced through the Town's Departments of Planning and Development and Public Works where public financing is involved.

Sandy Bottom Road Today





1.1 Overall Intent

- The purpose of the Route 3 and Sandy Bottom Road focus, in compliance with the goals of the Comprehensive Plan, is to revitalize and develop the Tiogue Avenue / Nooseneck Hill Road region so as to significantly improve the functionality and appearance of this arterial corridor, create and encourage redevelopment opportunities and diversity, generate employment and enhance the Town's economic tax base.
- With a focus on traditional, New England style architecture and Smart Growth site design, it is the Town's goal to create an aesthetically-pleasing and functional destination through collaboration with developers, engineers, architects, other professionals and citizens at large.
- The General Business Design Guidebook provides design direction for:
 - The design framework and design review process along commercial corridors in Coventry.
 - Approaches to execute the guidelines set forth in the document.
- This new Town-wide policy document will be a comprehensive design document that takes into consideration the existing and anticipated realities of Coventry as escalating commercial and residential growth occurs within the Town.

"If we lose our rural character, natural treasures and urban vitality, the Rhode Island Economic Policy Council and others now realize we lose one of our best business development assets. What sells Rhode Island to outsiders after all is quality of life, not quantity of asphalt." --Grow Smart RI

1.2 Background

- Commercial 'strip' development, commonly and inappropriately linked to market forces, is in reality, a zoning concept derived from an outdated zoning model involving segregation of land uses adopted years ago by local governments.
- The resulting negative effects of strip development often include increased traffic congestion, little connectivity between neighborhoods and an absence of 'sense of community'.
- In recent years, planners, economic development advisors and policy makers in North America have begun a shift away from conventional zoning practices which concentrated on prohibiting certain forms of land use, to zoning techniques which facilitate modern day needs, encourage creativity in urban design and recreate a sense of community in neglected areas.
- The 2000 Sustainable Communities Report for the Town of Coventry, based on a Sustainable Community Survey distributed among local Board and Commission members, staff and local citizens stressed the importance of '*General Quality of Life*' for residents. Survey participants were strongly in favor of the "*maintenance of secure, safe streets free from crime and traffic congestion*". Many indicated a longing for the "*sense of community that seems to be eroding as the Town grows.*"
- The General Business Design Guidebook incorporates ideas and goals set forth in the 2000 Sustainable Communities Report, the Comprehensive Community Plan and other policy documents geared to improving the quality of life for residents, business owners and visitors at the neighborhood scale.



"Anywhere, U.S.A."



1.3 How To Use This Document

- This document is to be used by anyone involved in the process of (re)designing and reviewing a commercial or mixed use subdivision, land development or development plan proposal located within the Tiogue Avenue and Sandy Bottom Road commercial corridors in the Town of Coventry, Rhode Island.
- All new commercial uses, changes in commercial uses or expansion of commercial uses located within the target area shall be subject to the General Business Design Guidelines derived from Article 530 of the Coventry Zoning Ordinance, in addition to any other applicable Zoning Ordinance, Subdivision and Land Development Regulations. Such uses should be reviewed by the Planning Department and/or Planning Commission as appropriate.
- Exceptions to the guidelines will be considered for a distinct landmark building or historical feature which serves a unique function or contributes to the community's use at large.
- This guidebook is to serve as a supplementary document to the Town of Coventry Zoning Ordinance and Subdivision and Land Development Regulations.
- It is recommended that each applicant speak or meet with representatives of the Coventry Planning Department prior to designing a site plan.



1.3 How To Use This Document

- The applicant should use this document for guidance in the design or redesign of a site within the Special Management District.
- The Town should use this document to ensure that the design principles adopted by the Town are reflected in the design elements of a site during the application process.
- The Town may also use this document to promote and attract outside businesses to the community.
- Within the Urban Design Guidebook document itself, the user will first examine Appendix B – Town of Coventry Route 3/Sandy Bottom Road Special Management District and or Appendix C – Route 3/Sandy Bottom Road Special Management District Plat/Lot Listing to determine if the subject property is affected by the Urban Design Guidebook. If the property is within an area highlighted on the map, the user will proceed to integrate the design preferences and recommendations set forth herein.

2.0 NEW ENGLAND TRADITIONAL ARCHITECTURE

- It is recommended that new buildings, redesigned buildings and overall site design take on the feel of an earlier, colonial style era, indicative to traditional New England.
- The architectural (re)design of buildings, structures and site layout should be visually compatible with the traditional historic character of Coventry and the surrounding area, including building materials, scale, landscaping and roof line.



Coventry Police Station



Gas Station in New Hampshire



Walgreen's in New Hampshire



Former Dolby's Ice Cream building, Washington Village, Coventry

3.0 DESIGN PRINCIPLES

There are four guiding principles – Accentuate Sense of Community, Promote Smart Growth Values, Integrate Context and Promote Creativity and Innovation. The Design Principles apply to all urban development along Tiogue Avenue and Sandy Bottom Road.

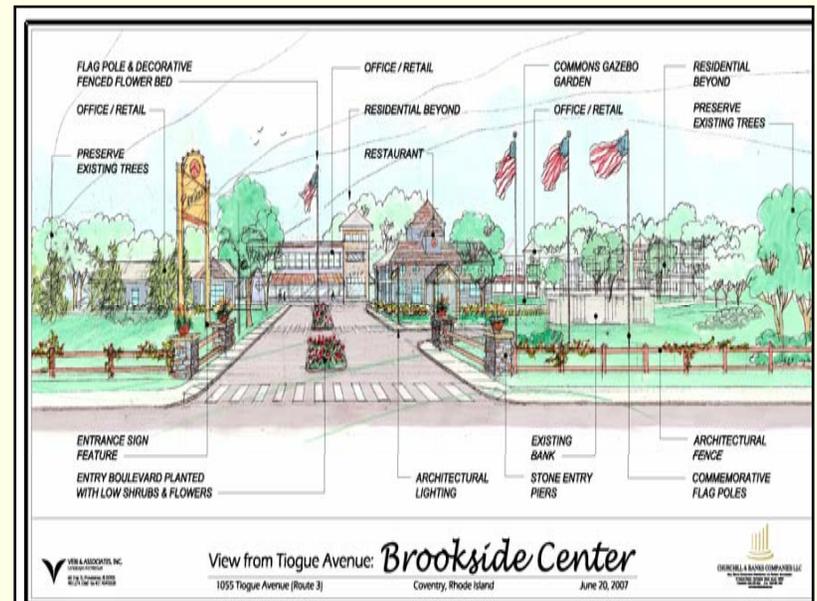
3.1 Accentuate Sense of Community

A sense of community is at the root of all efforts to build, revitalize and strengthen a community. It comes from a clear sense of collective purpose that values diversity and uniqueness. With current trends of urban sprawl, urban decay and increased mobility, many residents no longer feel strongly connected to their communities. Communities exhibiting a strong sense of community, on the other hand, are more attractive for economic development, tourism and are commonly linked to heightened sociability, safety and comfort.

Contributions to sense of community include (but are not limited to):

- Uniform landscaping
- Community newsletters and events
- Balanced mix of uses
- Uniform signage
- Pathway systems
- Entry features

Commercial development along Tiogue Avenue and Sandy Bottom Road, while embraced by Town Officials and residents alike, has centered primarily on automobile efficiency and accessibility leaving little consideration for “sense of community”. The goal of the guidelines is to encourage and support efforts to plan development, based on Smart Growth planning practices, in a way that promotes a sense of community and is sensitive to cultural, environmental and historical resources. The objective is to recreate a strong, distinct, positive identity for the Special Management District.



A sense of community is strong in this proposal for the old Almac's Shopping Plaza on Route 3 in Coventry. Opportunities for social interaction are created through safe, pedestrian-friendly public spaces and mixed use development.

Design Principles

3.2 Promote Smart Growth Values:

A collaborative effort by Town Officials, private developers, engineers and architects, on the following Smart Growth design values could substantially improve the local tax base, functionality and physical character of Route 3 and Sandy Bottom Road.

- Combine land uses;
- Take advantage of compact building design;
- Create a range of commercial and housing opportunities and choices;
- Create pedestrian friendly streets and neighborhoods;
- Cultivate distinctive, attractive communities with a strong sense of place;
- Preserve open space, natural beauty, and critical environmental areas;
- Provide a variety of transportation choices (e.g. Bicycle, bus);
- Encourage community and stakeholder collaboration in development decisions.

Tiogoe Avenue Today:



Desired Outcome:



Bethesda, Maryland

Design Principles

3.3 Integrate Context

The goal of this guideline is to create and help envision contextual sub-principles of urban design for Tiogue Avenue and Sandy Bottom Road based on small Town character. The Design Guidelines integrate context through:

- Places that visually and physically connect the natural and built heritage with all people;
- Planned development, with regard to how the site design impacts the environment and quality of life of the Town's citizens.



Design Principles

3.4 Promote Creativity and Innovation

Design guidelines are intended to inspire people to think outside the realm of the local zoning ordinance to create a development that is not only substantially compliant with Town regulations but also strengthens a community's value and character.

Design review should promote the creative application of design principles to a specific site which comport with the general guidelines of the Special Management District and embrace different solutions to general criteria. The Town needs to show its vitality by enabling opportunities for creative expression.

The strip development theme along Tiogue Avenue and Sandy Bottom Road has resulted in a lack of creativity and innovation in building design, streetscape and overall aesthetic appeal. With a renewed focus on traditional New England architecture, neighborhood beautification and Smart Growth principles, the aesthetic and functional nature of Tiogue Avenue and Sandy Bottom Road can be enhanced significantly.

Dave's Marketplace, a grocery store with a 'small town' feel and innovative design, is coming soon to Route 3



Dave's Marketplace, Wickford, RI

"If how we grow and where we grow doesn't change, we will lose our long-term capacity to advance economically as we become one continuous, unattractive, congested suburb with decaying inner cities." --Grow Smart RI



4.0 DESIGN GUIDELINES

4.1 Mixed Use Development

- **Objective:** To capture trips that would otherwise end up on peripheral streets, to increase the walkability of streets for local residents and visitors and to enhance the fiscal impact for the Town. To create a stronger sense of community through the provision of a variety of uses including commercial, civic and residential.
- **Guideline:** Mixed use, storied buildings (ie. commercial on ground floor, residential on subsequent floors) and mixed use, planned complexes such as Brookside Center (right) are encouraged for diversity and smart growth promotion. The developer should maximize opportunities for people to live in mixed income, mixed use developments and make shopping centers and business parks into all-purpose activity centers.

Existing Condition of Site Proposed for Brookside Center, Route 3



Proposed Brookside Center Mixed Used Development, Route 3



Veri & Associates, Inc., DiPrete
Engineering Associates, Inc., Thomas
Bakalars Architects, Pc.



4.2 Architectural Design

4.2.1 Franchise Architecture

- **Objective:** To recreate a unique identity for Tiogue Avenue and Sandy Bottom Road, characteristic of historic New England. To move away from the 'Big Box' phenomenon in order to create/recreate buildings with visual appeal.
- **Standard:** Franchise architecture is not permitted. Franchises or national chains should follow the Design Guidelines to create a unique building that is compatible with the goals and objectives of the Special Management District.

Typical McDonalds, Route 3, Coventry



McDonalds, Stowe, Vermont
Traditional New England theme



Preferred Dunkin Donuts Design
Salem, Massachusetts



Typical Burger King, Route 3,
Coventry



Burger King, Chesterfield, Virginia
Developed in accordance with the
Chesterfield Design Guidelines



4.2.1 Franchise Architecture

Typical Convenience Store



Convenience store, Charlottesville, Virginia
Developed in accordance with Albermarle County's design review guidelines



Typical Donut Shop, Route 3, Coventry



New England style Tim Hortons,
Route 3, Coventry



Typical Pizza Hut

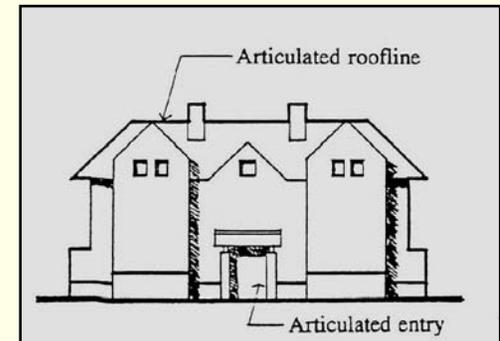


Aesthetically pleasing Pizza Hut,
Worcester County, Maryland

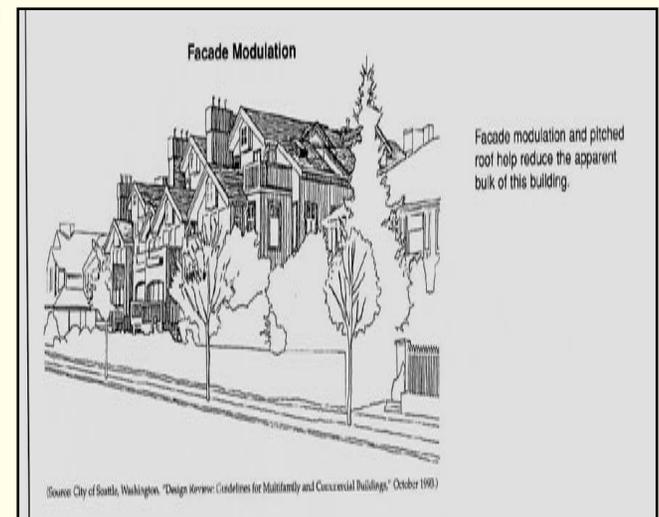


4.2.2 Modulation and Articulation

- **Objective:** To reduce the perceived size of large buildings and enhance their architectural character.
- **Guideline:** Modulate facades by extending or moving back a proportion of the façade for each interval of a building. Highlight specific architectural elements such as windows, entries and balconies (etc.) that create a harmonizing pattern, dividing large buildings into smaller, identifiable components. Architectural elements including dormers, columns, facades, awnings and canopies (etc.) should be in proportion to the dimensions and design of the overall building.
- **Standard:**
 - Exaggerated or excessively large architectural elements should be avoided.
 - A building more than 45 feet in width should be divided into increments of no more than 45 feet through articulation of the façade. This can be achieved through combinations of the following techniques:
 - Divisions or breaks in materials
 - Window Bays
 - Separate entrances and entry treatments
 - Variation in roof lines
 - Building setbacks (slight variations)

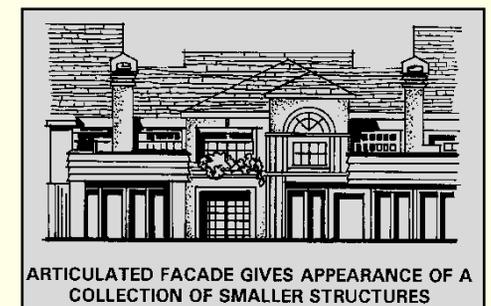
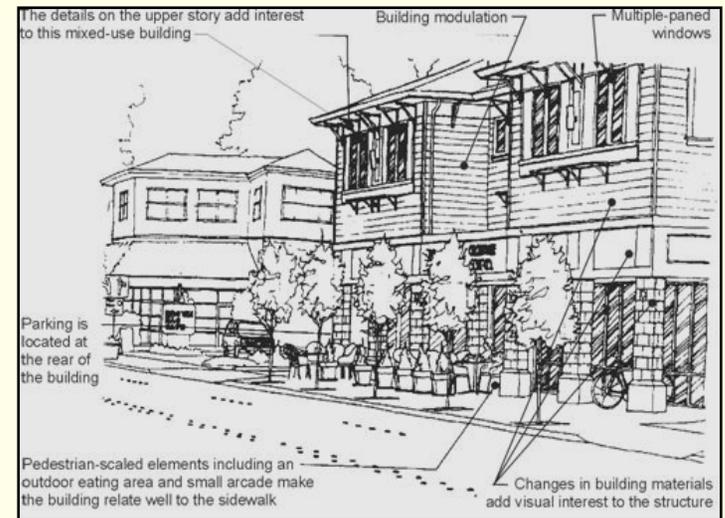


Large, windowless facades are strongly discouraged.



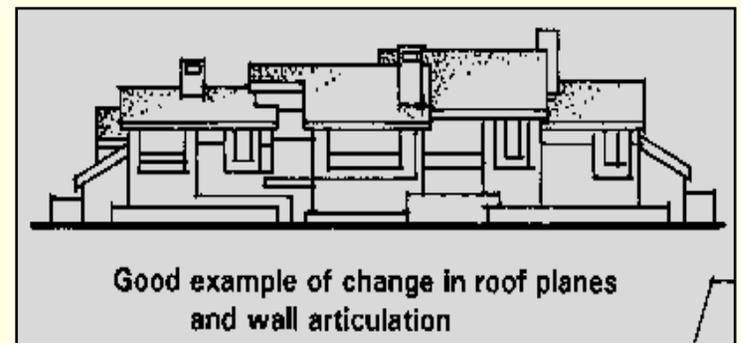
4.2.2 Modulation and Articulation

- All sides of a principal building which directly face abutting streets should include at least one customer entrance.
- Install or provide all facades that are visible from adjoining properties and/or public streets which encourage community integration by featuring characteristics of the front façade.
- Stairway construction should relate to that of surrounding buildings
- New building facades should conform to horizontal and vertical division systems used historically in New England.
- New construction should maintain the alignment of facades along the sidewalk edge wherever possible. Exceptions can be made if the setback is pedestrian-oriented and contributes to the quality and character of the streetscape (ie. outdoor patio seating).



4.2.3 Roofs

- **Objective:** Rooflines should be complementary to each other along Sandy Bottom Road and Route 3 and convey the character of an earlier era.
- **Guideline:** New buildings should be designed with traditional roof forms that are compatible with other traditional New England Towns.
- **Standard:**
 - Flat roofs should be avoided unless approved by the Planning Department.
 - Gabled, hipped and pitched roofs are highly recommended.
 - Green roofs should be considered to minimize storm water runoff.
 - Separations, changes in plane and height and the inclusion of elements such as balconies, porches, arcades, dormers and cross gables mitigate the barrack-like appearance of roofs of excessive length.
 - Secondary hipped or gabled roofs covering the entire mass of a building are preferable to mansard roofs or segments of pitched roof applied at the structure's edge



Good example of change in roof planes and wall articulation

4.2.4 Windows and Entries

- **Objective:** Improve building interest and safety by reducing the effects of lengthy, featureless facades and building walls.
- **Guideline:** New construction and reconstruction should provide windows and entrances or other features along public and private street frontage and pedestrian access ways meeting the requirements of this section.
- **Standard:**
 - Windows should be provided on the street level instead of blank walls to encourage a visual and economic link between the business and passing pedestrians.
 - At least 75 percent of the width of any new or reconstructed first-story building wall facing a street should be devoted to interest-creating features, pedestrian entrances, transparent show or display windows, or windows affording views into retail, office or lobby space.
 - Rear entrances should be clean and well-maintained. A small sign, awnings, display windows and planter boxes can improve the appearance of rear entries.
 - Minimize windows which overlook adjacent residences.
 - Patterns and proportions of windows should be consistent with surrounding buildings.
 - Avoid long, blank exterior walls.
 - Storefront facades should consist of no less than 60% non-reflective, transparent glass display windows with trim unless alternative proposal is provided accomplishing the same interest-creating intent with compatible architectural treatments.
 - Tinted and mirrored windows should be avoided.
 - Large windows are encouraged for visual appeal.
 - See also Energy Efficiency section for further details related to window placement.



Inviting, well-maintained rear entries



4.3 Site Design

4.3.1 Transit, Pedestrian & Bicycle Site Design

- **Objective:** To promote neighborhood connectivity and energy efficiency by providing better opportunities for public transportation, pedestrian and bicycle travel. A better transportation system promotes economic development.
- **Guideline:** An on-site pedestrian and bicycle circulation system that connects the street and the primary entrances of the structure(s) on the parcel should be provided. Pedestrian pathways, bike paths or sidewalks should connect the required pedestrian system to existing pedestrian systems on bordering developments if adequate safety and security can be sustained. Pedestrian access to transit stops should be provided where convenient.



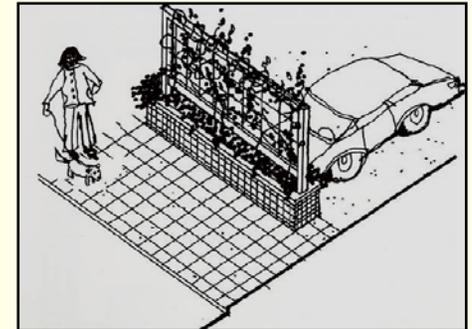
The 2000 Coventry Comprehensive Community Plan sets forth the following goals related to circulation:

- "Work to ensure the provision of functional and affordable public transit service to Coventry Residents"
- "Develop and improve non-motorized circulation throughout the Town of Coventry"

4.3.1 Transit, Pedestrian & Bicycle Site Design

- **Standard:**
 - Expand upon the number of transit stops and shelters and increase route frequency within the Special Management District
 - Transit shelters should include benches, trash containers, clear signage and landscaping
 - Pedestrian and bicycle pathways that abut parking lots should be physically separated from driveways and parking spaces by landscaping, berms, barriers, grade separations or other means to protect pedestrians from vehicular traffic and enhance visual appeal.
 - Provisions for bicycle travel, including bike racks and on-site lockers and showers should be incorporated into commercial developments creating more than 15 jobs.
 - Pedestrian crosswalks (raised or brick pave) should be necessary when a walkway crosses a driveway or paved area accessible to vehicles
 - Brick paves or other materials approved by the Planning Department should be used to signify pedestrian crossings
 - Corner bump-outs and parking bays should be used to enhance pedestrian safety
 - Shared driveways are encouraged where feasible to reduce the number of curb cuts and in turn enhance safety.

Suggested Screening for Visibility and Security:



Source: *Creating Transit-Supportive Land-Use Regulations*
Edited by: Marya Morris, APA Publication



Pedestrian access to transit stops

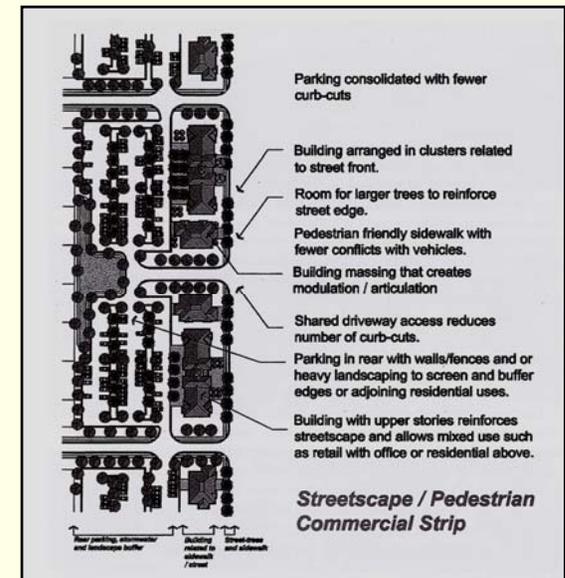
4.3.2 Parking Lots

- **Objective:** Parking should be adequate but should not exceed the number of spaces dictated by Article XII of the Town's Zoning Ordinance.
- **Guideline:** Consideration may be given to granting relief from the required number of parking spaces in any given development in exchange for a well-landscaped, pedestrian-friendly proposal. The inclusion of open space or small parks within private development proposals is strongly encouraged.
- **Standard:**
 - Parking lots constructed behind buildings are preferred to parking lots directly overlooking Route 3 or Sandy Bottom Road.
 - Parking lots should include low walls/fences and/or heavy landscaping around the perimeter to adequately screen and buffer abutting uses.



Parking Lot in rear of building

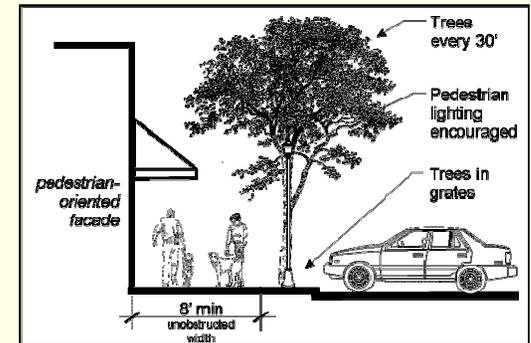
Recommended Design:



Source: *Redesigning Cities*
by Jonathon Barnett, APA Publication

4.3.3 Sidewalks

- **Objective:** To enhance the pedestrian environment, improve overall safety and connectivity of Coventry neighborhoods, and create streetscape opportunities along Sandy Bottom Road.
- **Guideline:** Sidewalks should be installed along both sides of Sandy Bottom Road in coordination with the Town Engineer and Rhode Island Department of Transportation. Appropriate curbing, as determined by the Town Engineer should also be installed.
- **Standard:** All sidewalks shall be constructed of concrete with wire mesh and be a minimum of four (4) feet wide.



Desired:



Existing:

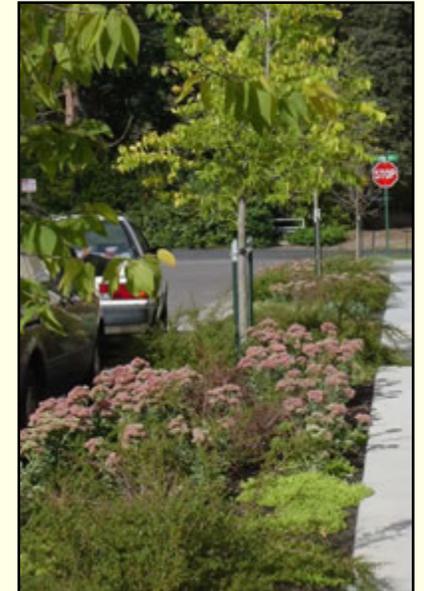


Sandy Bottom Road today lacks sidewalks and streetscape

4.3.4 Environment and Landscape

- **Objective:** Landscaping should be utilized to enhance the pedestrian experience, complement architectural features, provide buffers to residential districts and/or screen utility areas and parking lots. Landscaping plans within the Special Management District should be complimentary to each other for continuity.
- **Guideline:** A visually pleasing and functional relationship should exist between the three dimensions of length, width and height. Wherever possible, natural and historic features should be preserved.. Refer to Article 17 of the Coventry Zoning Ordinance for further details pertaining to landscaping.
- **Standard:**
 - Artificial plants such as grass and flowers should not be utilized as landscaping features.
 - Landscaping should be drought resistant and focus on a variety of deciduous and coniferous native plant species for year round coverage.
 - Stone or gravel, in place of organic mulch in landscape plans is not permitted.
 - Evergreen trees should be placed on all sides of a building that abut or face a residentially-zoned property.

Denver, Colorado



Smaller lot sizes and native, drought tolerant landscaping in tree wells and gardens reduce demand for water.



Artificial grass and flowers and lack of dimension on Sandy Bottom Road, Coventry



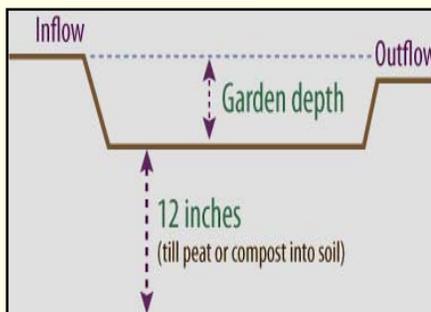
Well maintained, attractive landscaping enhances visual appeal

4.3.4 Environment and Landscape

- Groundcover, landscaping beds and trees should be incorporated into parking lots of all sizes. (ie. Ocean State Job Lot, Super Stop and Shop, Benny's, gas stations, liquor stores, etc.)
- Evergreen trees should be placed in front of expansive and windowless facades for screening.
- Shade trees should be preserved and/or planted near buildings, particularly on east and west exposures to block the sun in the morning and late afternoons when it is low in the sky.
- The use of flower boxes, planters and hanging flower baskets is encouraged to break up the appearance of large paved areas.
- Rain gardens can be used to mitigate stormwater runoff and soil erosion on a site.



Typical Rain Garden Design
See www.rainkc.com for more details



Rain garden

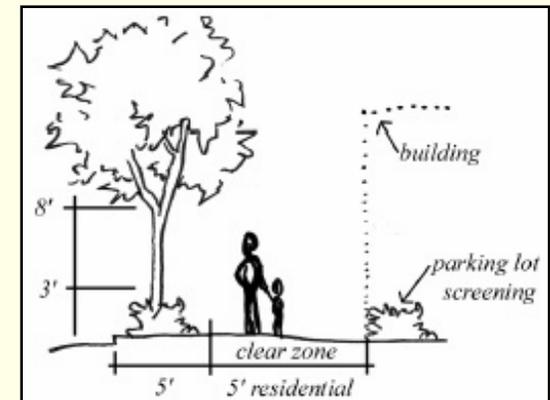


Suitable large façade screening



4.3.5 Street Trees

- **Objective:** To enhance the visual appeal of Route 3 and Sandy Bottom Road for both the driver and the pedestrian and to create a buffer between streets/driveways and sidewalks for enhanced pedestrian safety.
- **Guideline:** Street trees should be planted on all street frontages (including driveways and private roads) and median planting strips. Refer to Article XIII of the Coventry Zoning Ordinance for further details pertaining to the installation of street trees. Implementation of street trees is a developer responsibility that applies to new developments where no trees exist currently or to replace trees that have been removed during the development process.
- **Standard:**
 - Street trees should be spaced no further than 30 feet on center.
 - Street trees shall be drought resistant and consistent in theme and species with surrounding street trees and landscaping.
 - Tree well openings should have a minimum pit size of two times the width of the root ball and depth equal to the height of the root ball.
 - Decorative grates should be used at the base of all street trees (see Streetscape section).



Street trees located between the walking route of the sidewalk and curb edge.



Shrubs and trees used between pedestrian and vehicular traffic

4.3.6 Screening for Utility Areas & Mechanical Equipment

- **Objective:** To hide objectionable elements of building design from public view.
- **Guideline:**
 - Utility areas and mechanical equipment (ie. trash and storage areas, loading areas, air conditioning units, etc.) should be screened using appropriate landscaping or the same materials, color and/or style as the primary building in order to be architecturally compatible with adjacent buildings.
 - Utility areas and mechanical equipment should be located away from the street side of a building.
 - Utilities should be located so as to avoid conflict with the pedestrian environment.



Landscaping can be used to screen utility areas from view

4.4 Energy Efficiency & Green Design

- **Objective:** To protect the environment for future generations by reducing the demand on natural resources and minimizing the impact of fossil fuel electric power generation. To encourage sustainable design.
- **Guideline:** New buildings and redeveloped buildings should take steps to become LEED (Leadership in Energy and Environmental Design) Certified. Existing and new buildings within the Special Management district should incorporate energy saving technology into their (re)design.
- **Standard:**
 - Windows and doors with R-values of at least two (2) are encouraged.
 - Consider solar energy as an alternative to conventional sources of energy.
 - Choose Energy Star appliances to outfit businesses and residential dwellings.
 - Install high efficiency heating and cooling systems.
 - Plant trees on the North and West sides of a building to reduce winter winds.
 - Deciduous trees should be planted on southern elevations to provide shade during the summer but let sun shine through during the winter months.
 - Ensure that there are a number of windows installed on the south elevation to maximize heat and light absorption in the winter months.
 - Use energy-efficient construction methods and materials.
 - Use compact fluorescent light bulbs for interior and exterior lighting.
 - Provide extensive landscaping and green space and minimize paved areas.
 - Used recycled building materials wherever possible.
 - Implement 'green' roofs to mitigate storm water run-off.



Energy Star Certification is a basic requirement of the LEED Certification for homes





4.5 Streetscape

4.5.1 Streetscape Furniture

- **Objective:** To develop a consistent streetscape theme that incorporates design elements that are reminiscent of the Town's past. To design a streetscape that is inviting, safe, accessible, and emphasizes the ground floor and façade of buildings.
- **Guideline:** Provide quality pedestrian-friendly amenities along public and private walks. Every effort should be made to incorporate inviting community and public spaces such as courtyards, gathering places, seating and rooftop gardens into private development. Streetscape amenities such as benches, trash receptacles, planters, pole lights, kiosks, telephones, clock towers, new stands, drinking fountains, water fountains, and bike racks liven up and sustain the public realm. Cafe tables in the right-of-way can trigger activity and socialization in the area. Streetscape features should be consistent to ensure continuity of style. Street furniture design should consider the ease of use by persons with disabilities, frequency of maintenance, durability, aesthetics and permanence of use.
- **Standard:**
 - Lawn ornaments/figurines are not permitted within the Special Management District unless approved by the Planning Department.
 - Decorative grates should be used at the base of street trees.
 - Decorative but durable benches and trash receptacles appropriately placed should be utilized to enhance the social atmosphere of all developments.
 - Landmark features should be constructed of durable materials and be in keeping with the scale of surroundings. It is the developer's responsibility to maintain landmark features.

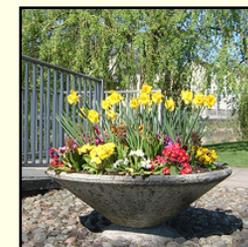
Acceptable
Decorative
Tree Grate



Acceptable
Street Bench



Acceptable
Street Planters



4.5.1 Streetscape Furniture



Outdoor seating can help create neighborhood identity



Sample bike rack



Hanging baskets on lampposts add an attractive streetscape element



Acceptable waste receptacle



Banners and artwork help create sense of place

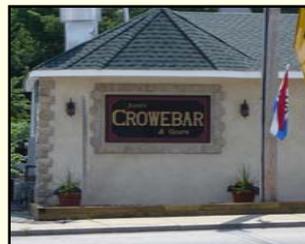
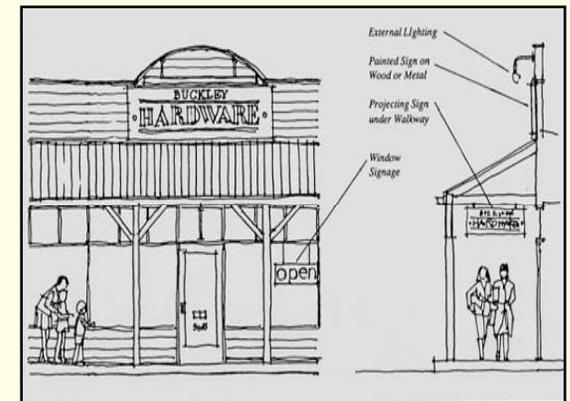


Gathering Spaces promote social interaction

4.5.2 Signs

- **Objective:** Signs should be architecturally compatible with the style, composition, materials, colors and details of the building and with other signs on nearby buildings, while providing for adequate identification of the business.
- **Guideline:** Symbolic and historic three-dimensional signs such as appropriately-sized projecting signs are encouraged. Signs constructed of natural materials such as metal or wood are preferred. Lettering which recreates the feel of an earlier era should be utilized. Permanently painted window signage is encouraged if compatible with the architecture of the building. Painted window signs should not consume more than one-third of the window surface area. Externally lit signs are preferred to internally lit or neon signage. Integrated signage assists with a building's identity.
- **Standard:** Internally lit and electronic signs, and pylon and monument signage are strongly discouraged. Temporary window signage is limited to one-third of the window surface area. Projecting signs should be no greater than 12 square feet (this can be revised and approved by the Planning Department as appropriate) and have a maximum width of three feet and should not extend beyond the first floor of the building. No less than 10 feet of clearance should be provided between the sidewalk elevation and the lowest point of the projecting sign. Signs cannot block or obliterate design details, windows or cornices of the building upon which they are placed.
For safety and aesthetic purposes, signs attached to the inside or outside of storefronts should be kept to a minimum.

Desired signage:



Desired signage:

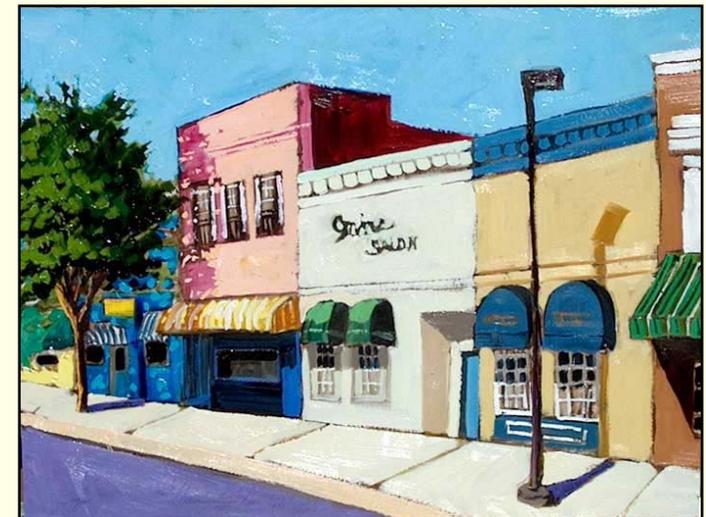
4.5.2 Signs

Examples of unacceptable signage:



4.5.3 Awnings

- **Objective:** To enhance the character of Tiogue Avenue and Sandy Bottom Road and provide sun protection for display windows, shelter for pedestrians, and a sign panel for businesses.
- **Guideline:** Fixed awnings composed of wood or metal are encouraged. Long expanses of awning should be broken into segments that reflect the door or window openings beneath them.
- **Standard:** Awnings should not extend across multiple storefronts and/or multiple buildings. Awnings should be constructed of durable, protective and water repellent material, however, plastic or fiberglass awnings are not acceptable. Backlighting or illuminating awnings are not permitted. Awnings should project a minimum of 36" from the building.



4.5.4 Finish Materials & Color

- **Objective:** To promote the use of traditional, durable building materials and to encourage a varied but complementary use of color.
- **Guideline:** Buildings should be detailed with materials that vary between base wall material and trim. Exterior finishes should be durable commercial applications of traditional materials including wood, stone, brick, or stucco. Trim and detailing should be composed of wood moldings and trim, decorative brick trim or stone trim.

Exterior color schemes should include contrasting base and trim colors. The color theme should be a “New England” theme or any color scheme included in any paint manufacturer’s historic paint line or the like reflecting a historic theme. Façade colors should be low reflective, subtle, neutral or earth-tone colors.

- **Standard:** High intensity or metallic colors on a building’s exterior are prohibited. Neon tubing as an accent material is not permitted. Vinyl siding, cinder blocks and other materials lacking historical character should be avoided as exterior finishes.

Desired outcome:



Garden Center, Route 3, Coventry

4.5.5 Lighting

- **Objective:** Lighting within the Special Management District should illuminate building facades, entrances, signage, parking areas, changes in grade and walkways and provide an adequate level of personal safety while simultaneously enhancing the visual character of the site.
- **Guideline:** Lighting for parking lots and pedestrian ways should be provided to ensure individual safety. Lighting should be incorporated into the architectural character both in terms of illumination and fixtures. Lighting should not generate glare or negatively impact off-site uses, naturally sensitive areas or traffic on adjacent streets.
- **Standard:**
 - Neon tubing is prohibited as a light fixture within the Special Management District
 - In order to achieve continuity in building and site lighting, colored lighting schemes should be avoided.
 - Decorative street lamps, illuminated bollards, underground lighting and/or globe lighting is recommended for site design.
 - Decorative street or internal lighting should be consistent with that of abutting properties.
 - Exterior lighting should be indirect and screened by shields or hoods to prevent glare onto adjacent properties and environmentally sensitive areas.
 - Excessive, harsh or insufficient lighting, as determined by the Planning Department should be avoided.
 - Lighting in parking areas should provide less intense levels of light by using a larger number of smaller poles ranging in height from 12 feet to 18 feet.

Historic Benefit Street,
Providence

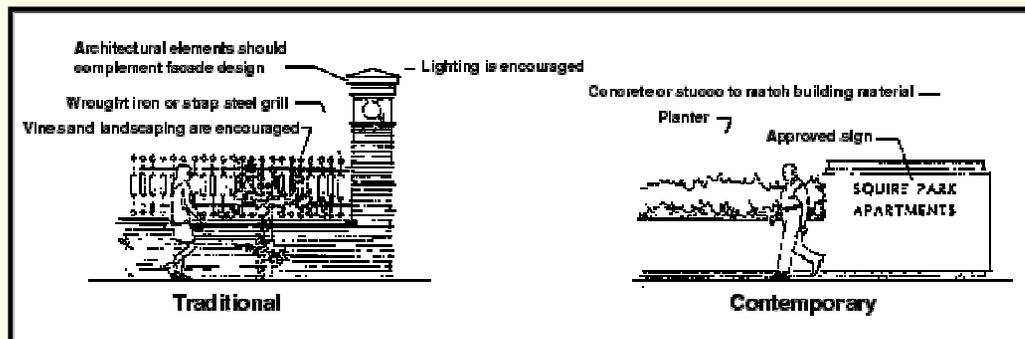
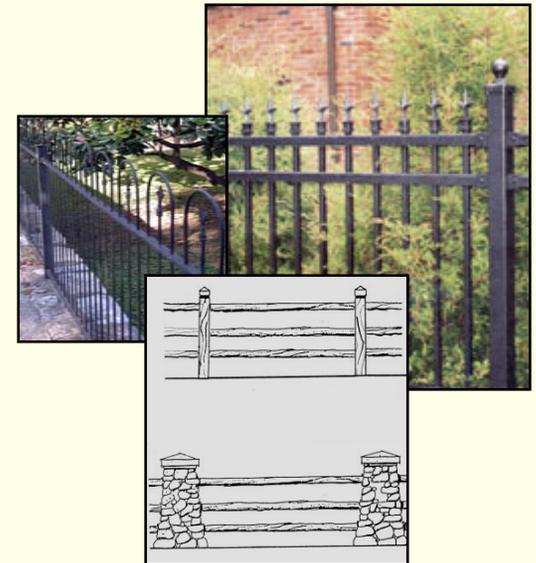


Decorative lamp posts add character and
enhance safety

4.5.6 Fencing

- **Objective:** To enhance pedestrian safety and provide a visually appealing border around development.
- **Guideline:**
- **Standard:** Freestanding walls, fences and hedges may be installed along public and private streets if they meet the following requirements:
 - Durable, wood, stone or metal low walls, low fences and low iron gates are encouraged
 - Solid walls, fences and hedges four feet high or less are permitted
 - Decorative walls, fences and hedges that allow visibility, such as wrought iron, and split rail fences, should be allowed throughout the Special Management District. Fencing and hedges should be reviewed by the Planning Department and/or Town Engineer for sight distance clearance.
 - Barbed wire, chain link, razor wire, electric fences and other dangerous fences are prohibited in the Special Management District
 - All permitted walls, hedges, and fences between a building and public street should provide for access at least every 100 feet.

Suitable fencing



Chain link fencing should be avoided



A photograph of a forest floor with moss, small green plants, and tree trunks. The text "5.0 GLOSSARY" is overlaid on this image.

5.0 GLOSSARY



Glossary

- **Articulation:** Emphasis on architectural elements (ie. windows, balconies, entries. Etc.) that create a complementary pattern or rhythm, dividing larger buildings into smaller, identifiable pieces.
- **Big Box:** Large, box-like, chain retail stores, typically over 35,000 square feet drawing customers from a large area and typically surrounded by parking lots.
- **District:** A segment of a community identified on the locality's zoning map within which one or more primary land uses are permitted along with their accessory uses and any special land uses permitted by the zoning provisions for the district.
- **Franchise Architecture:** building design that is trademarked or identified with a particular chain or corporation and is generic in nature.
- **Green Roof:** A green roof is a roof of a building that is partially or completely covered with vegetation and soil, or a growing medium, planted over a waterproofing membrane. This does not refer to roofs which are merely colored green, as with green shingles. It may also include additional layers such as a root barrier and drainage and irrigation systems. The term "green roof" may also be used to indicate roofs that utilize some form of "green" technology, such as solar panels or a photovoltaic module. Green roofs are also referred to as eco-roofs, vegetated roofs, living roofs, and green roofs.
- **Mixed Use Development:** Allowance of a variety of uses in a single zoning district or neighborhood including but not limited to residential, employment, entertainment and shopping.
- **Modulation:** Proportioned inflexion or setbacks to reduce the appearance of a bulky building.
- **Public Space:** The parts of the town (owned publicly or privately) that are freely available for everyone to frequent including but not limited to parks, streets and squares.



Glossary

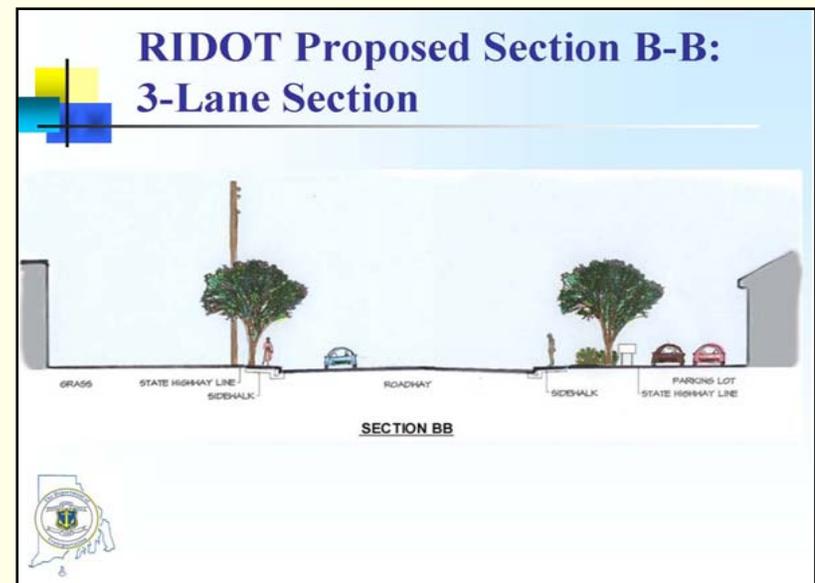
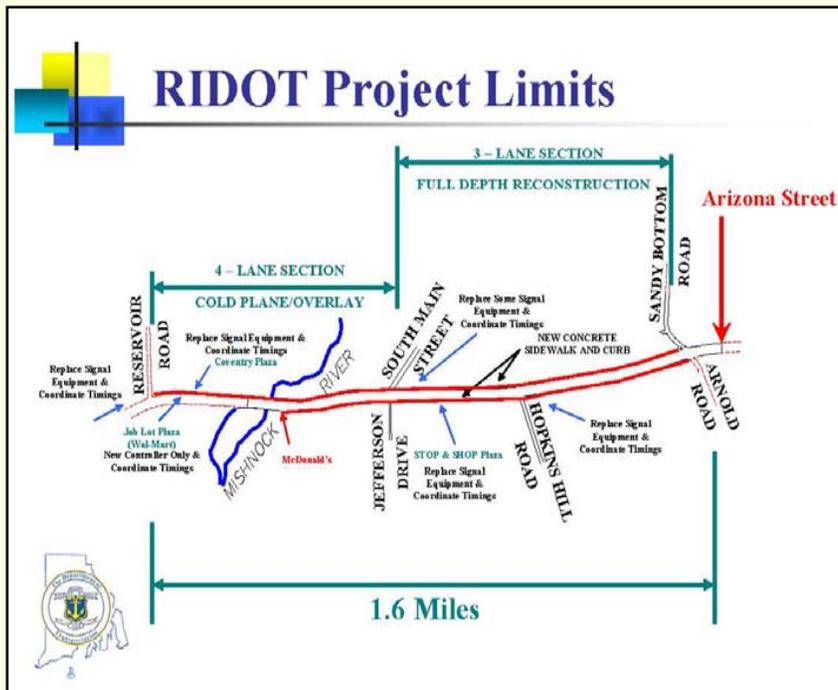
- **Rain Garden:** A rain garden is a shallow depression, planted with native plants, designed to slow, capture, and infiltrate rain. Rain gardens filter pollutants from stormwater run-off, lessen localized flooding during heavy storms, and enhance landscapes with native plants, but rain gardens also produce measurable economic benefits.
- **Sense of Community:** A sense of community is an intangible yet vital component of a healthy community. It encompasses elements such as image, spirit, character and pride, along with processes such as communication, inter-group relations, and networking. In many communities across the nation, people are losing their sense of community.
- **Smart Growth:** A set of principles that guide how neighborhoods, towns and cities are built or re-built based on long term sustainability and functionality. Smart Growth techniques feature compact, mixed-use, transit-oriented development with the objective of creating more attractive, livable, economically strong communities while protecting natural resources.
- **Traditional New England Architecture:** Three types of architectural styles are commonly linked to traditional new England. These include: Post Medieval English/Early Colonial Style, American Georgian and Federal Style. For examples of each type of architecture, please visit www.memorialhall.mass.edu/activities/architecture/index.html or consult a registered architect.
- **Tree Well:** Ground area surrounding the base of the tree trunk not covered by sidewalk and paving.



6.0 APPENDIX

Appendix A: RIDOT Capital Improvement Project for Route 3

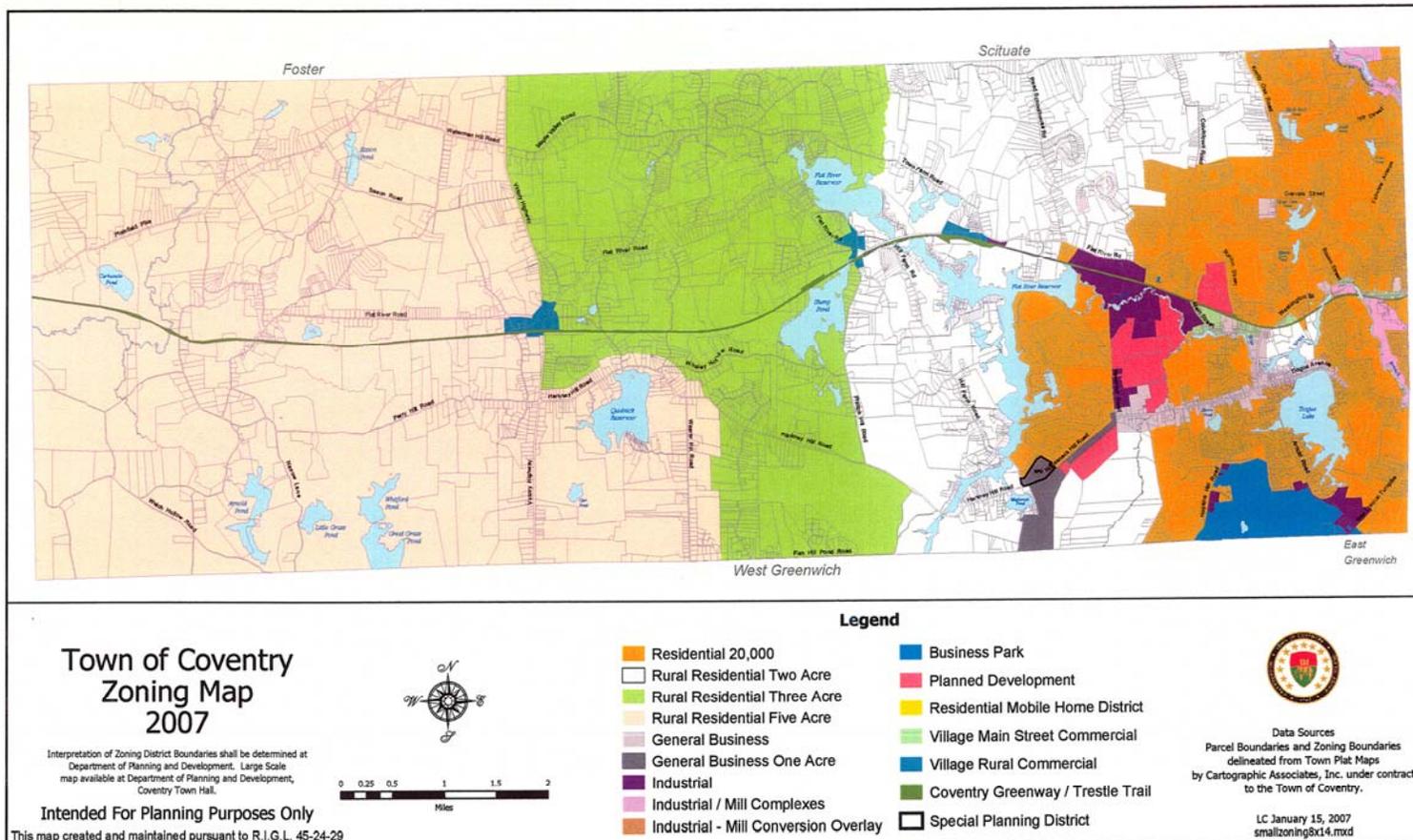
A separate streetscape contract will be advertised when the construction project is nearing completion.



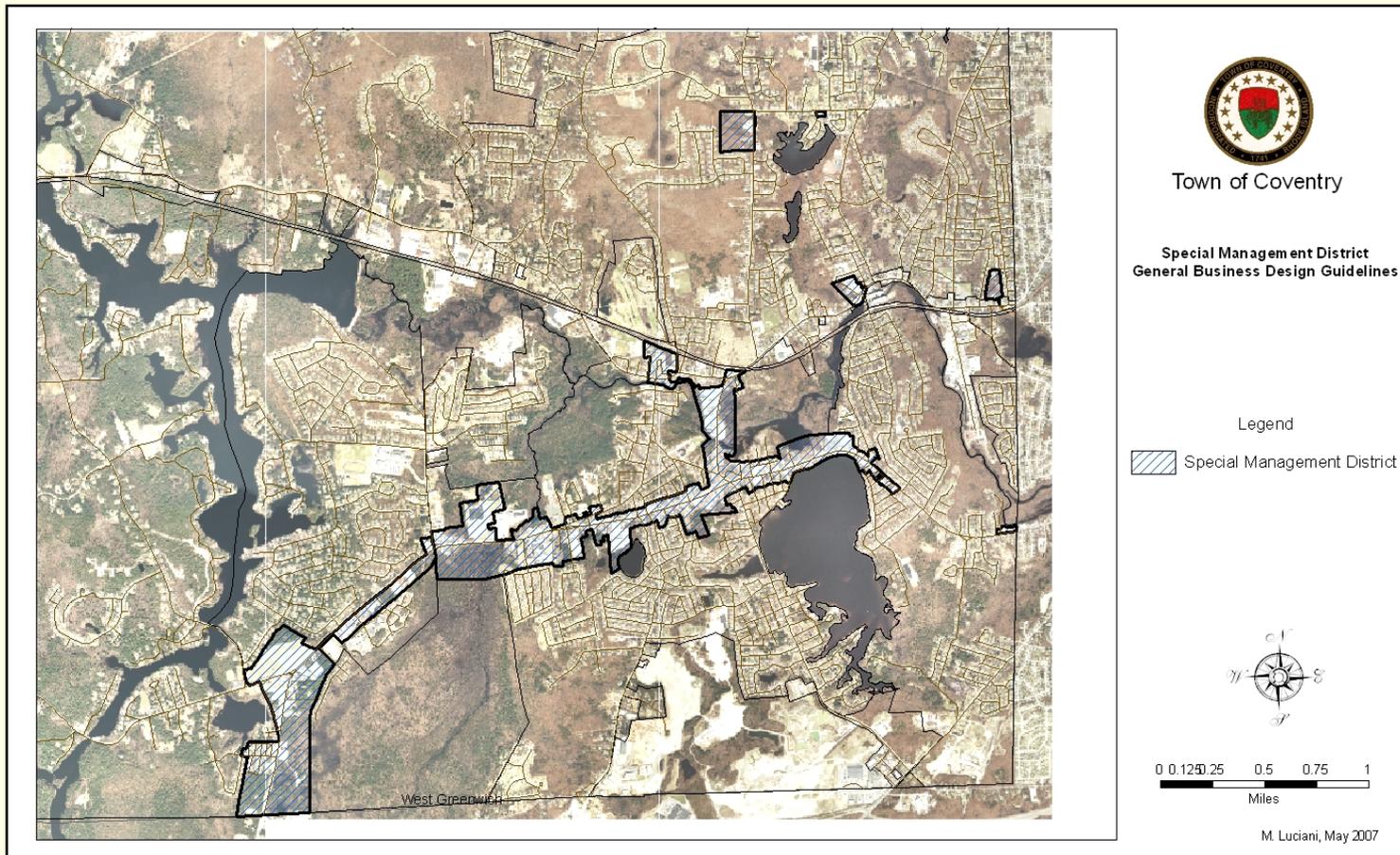
Sidewalk Plan:

- Both sides of Route 3 from Sandy Bottom Road to Mishnock River
- North Shoulder from Mishnock River to Reservoir Road
- Conceptual Plans have been submitted to extend the sidewalk from Reservoir Road to Harkney Hill Road (North shoulder)

Appendix B: Town of Coventry Zoning Map 2007



Appendix C: Town of Coventry Route 3/Sandy Bottom Road Special Management District



Appendix D: Route 3/Sandy Bottom Road Special Management District Plat/Lot Listing

| Special Management District Parcels May 2007 | | | | | | | | |
|--|-------|------|----------|-------|------|----------|--------|------|
| Plat Map | Lot | Zone | Plat Map | Lot | Zone | Plat Map | Lot | Zone |
| 2 | 3 | GB1 | 28 | 43.1 | GB | 29 | 38 | GB |
| 2 | 4 | GB1 | 28 | 43.2 | GB | 29 | 26 | GB |
| 2 | 5 | GB1 | 28 | 24.11 | GB | 29 | 30 | GB |
| 2 | 2 | GB1 | 28 | 1 | GB | 29 | 178.2 | GB |
| 2 | 1.2 | GB1 | 28 | 43.4 | GB | 29 | 178.3 | GB |
| 2 | 1.1 | GB1 | 28 | 44 | GB | 29 | 41 | GB |
| 10 | 29 | GB | 28 | 47 | GB | 29 | 25 | GB |
| 10 | 31 | GB | 28 | 48 | GB | 29 | 24 | GB |
| 10 | 32 | GB | 28 | 45 | GB | 29 | 40 | GB |
| 10 | 33 | GB | 28 | 49 | GB | 29 | 1 | GB |
| 10 | 41 | GB | 28 | 50 | GB | 29 | 23 | GB |
| 10 | 42 | GB | 28 | 51 | GB | 29 | 22 | GB |
| 10 | 40.11 | GB1 | 28 | 52 | GB | 29 | 178.1 | GB |
| 10 | 34 | GB1 | 28 | 40.3 | GB | 29 | 21 | GB |
| 10 | 35 | GB1 | 28 | 53 | GB | 29 | 206 | GB |
| 10 | 36 | GB1 | 28 | 40.4 | GB | 29 | 18 | GB |
| 10 | 40.2 | GB1 | 28 | 46 | GB | 29 | 207 | GB |
| 10 | 40.3 | GB1 | 28 | 50 | GB | 29 | 20 | GB |
| 10 | 40.12 | GB1 | 28 | 51 | GB | 30 | 161 | GB |
| 10 | 37.1 | GB1 | 28 | 50 | GB | 31 | 65.1 | GB |
| 10 | 37 | GB1 | 28 | 52 | GB | 32 | 147 | GB |
| 10 | 39 | GB1 | 28 | 40.5 | GB | 37 | 114 | GB |
| 10 | 38 | GB1 | 28 | 40.6 | GB | 37 | 113 | GB |
| 19 | 21 | GB | 28 | 51 | GB | 37 | 118 | GB |
| 27 | 116 | GB | 29 | 16 | GB | 37 | 117 | GB |
| 27 | 117 | GB | 29 | 19 | GB | 37 | 116 | GB |
| 28 | 3 | GB | 29 | 17 | GB | 37 | 119 | GB |
| 28 | 1.1 | GB | 29 | 229 | GB | 37 | 115.01 | GB |
| 28 | 21 | GB | 29 | 230 | GB | 37 | 120 | GB |
| 28 | 6 | GB | 29 | 231 | GB | 37 | 115 | GB |
| 28 | 20 | GB | 29 | 232 | GB | 38 | 126 | GB |
| 28 | 40.1 | GB | 29 | 233 | GB | 38 | 125 | GB |
| 28 | 41 | GB | 29 | 237 | GB | 38 | 175 | GB |
| 28 | 13 | GB | 29 | 238 | GB | 38 | 173 | GB |
| 28 | 12 | GB | 29 | 239.1 | GB | 38 | 127.1 | GB |
| 28 | 11.2 | GB | 29 | 239.2 | GB | 38 | 124.1 | GB |
| 28 | 9 | GB | 29 | 240 | GB | 38 | 127.2 | GB |
| 28 | 22 | GB | 29 | 242 | GB | 38 | 176 | GB |
| 28 | 5 | GB | 29 | 244 | GB | 38 | 128 | GB |
| 28 | 7 | GB | 29 | 24.12 | GB | 38 | 1.1 | GB |
| 28 | 23 | GB | 29 | 34 | GB | 38 | 124.2 | GB |
| 28 | 10 | GB | 29 | 33 | GB | 38 | 129 | GB |
| 28 | 4 | GB | 29 | 35 | GB | 38 | 172.2 | GB |
| 28 | 8 | GB | 29 | 32 | GB | 38 | 123.1 | GB |
| 28 | 25 | GB | 29 | 43 | GB | 38 | 140 | GB |
| 28 | 2 | GB | 29 | 36 | GB | 38 | 174 | GB |
| 28 | 26 | GB | 29 | 3 | GB | 38 | 130 | GB |
| 28 | 3 | GB | 29 | 2 | GB | 38 | 154 | GB |
| 28 | 42 | GB | 29 | 37 | GB | 38 | 1.2 | GB |

| Plat Map | Lot | Zone | Plat Map | Lot | Zone | Plat Map | Lot | Zone |
|----------|-------|------|----------|------|------|----------|------|------|
| 38 | 134 | GB | 38 | 94 | GB | 46 | 34.1 | GB |
| 38 | 2 | GB | 38 | 98 | GB | 46 | 32 | GB |
| 38 | 123 | GB | 38 | 93 | GB | 46 | 33.2 | GB |
| 38 | 171 | GB | 38 | 103 | GB | 46 | 37 | GB |
| 38 | 149 | GB | 38 | 102 | GB | 46 | 33.1 | GB |
| 38 | 3 | GB | 38 | 92 | GB | 46 | 42 | GB |
| 38 | 172 | GB | 38 | 99 | GB | 46 | 40 | GB |
| 38 | 143 | GB | 38 | 100 | GB | 46 | 41 | GB |
| 38 | 122 | GB | 38 | 86 | GB | 46 | 39 | GB |
| 38 | 139 | GB | 38 | 101 | GB | 46 | 38 | GB |
| 38 | 4 | GB | 39 | 67.1 | GB | 46 | 37 | GB |
| 38 | 172.1 | GB | 39 | 82 | GB | 55 | 20 | GB |
| 38 | 131 | GB | 39 | 71 | GB | 55 | 21 | GB |
| 38 | 5 | GB | 39 | 70 | GB | 55 | 16 | GB |
| 38 | 148 | GB | 39 | 68 | GB | 55 | 15 | GB |
| 38 | 144 | GB | 39 | 81 | GB | 55 | 14 | GB |
| 38 | 6 | GB | 39 | 75 | GB | 55 | 13 | GB |
| 38 | 138 | GB | 39 | 69 | GB | 55 | 11 | GB |
| 38 | 7 | GB | 39 | 73 | GB | 55 | 12 | GB |
| 38 | 147 | GB | 39 | 74 | GB | 56 | 38 | GB |
| 38 | 107 | GB | 39 | 67 | GB | 64 | 79 | GB |
| 38 | 137 | GB | 39 | 107 | GB | 70 | 65 | GB |
| 38 | 8 | GB | 39 | 65 | GB | 71 | 39 | GB |
| 38 | 46 | GB | 39 | 66 | GB | 79 | 98 | GB |
| 38 | 106 | GB | 39 | 49 | GB | 96 | 36 | GB |
| 38 | 146 | GB | 39 | 64 | GB | 96 | 38.2 | GB |
| 38 | 56 | GB | 39 | 50.1 | GB | 96 | 38.1 | GB |
| 38 | 16 | GB | 39 | 50.2 | GB | 96 | 40.1 | GB |
| 38 | 9.1 | GB | 39 | 62.1 | GB | 324 | 2 | GB |
| 38 | 65 | GB | 39 | 62.2 | GB | | | |
| 38 | 15 | GB | 39 | 57.1 | GB | | | |
| 38 | 47 | GB | 39 | 58.1 | GB | | | |
| 38 | 66.1 | GB | 39 | 29.1 | GB | | | |
| 38 | 15 | GB | 39 | 29.2 | GB | | | |
| 38 | 45 | GB | 45 | 21 | GB | | | |
| 38 | 55 | GB | 45 | 22 | GB | | | |
| 38 | 105 | GB | 45 | 27 | GB | | | |
| 38 | 76 | GB | 45 | 41 | GB | | | |
| 38 | 14 | GB | 45 | 26 | GB | | | |
| 38 | 77 | GB | 45 | 22.1 | GB | | | |
| 38 | 14 | GB | 45 | 25 | GB | | | |
| 38 | 66.2 | GB | 45 | 23 | GB | | | |
| 38 | 58 | GB | 45 | 24 | GB | | | |
| 38 | 64 | GB | 46 | 36 | GB | | | |
| 38 | 78 | GB | 46 | 30 | GB | | | |
| 38 | 75 | GB | 46 | 43 | GB | | | |
| 38 | 95 | GB | 46 | 35 | GB | | | |
| 38 | 96 | GB | 46 | 34 | GB | | | |
| 38 | 97 | GB | 46 | 45 | GB | | | |
| 38 | 104 | GB | 46 | 31 | GB | | | |

Please note: This list is subject to change. Contact the Department of Planning and Development to confirm the Zoning District in which a particular lot lies.

Appendix E: Article 530 – Route 3/Sandy Bottom Road Special Management District Zoning Ordinance

530 - SPD-Special Planning Overlay District/Special Management District for Route 3 and Sandy Bottom Road

531 Conformity with the Comprehensive Plan. All development along the Tiogue Avenue/Nooseneck Hill Road (Route 3) and Sandy Bottom Road corridors shall conform to the policies and guidelines as set forth in the Town's Comprehensive Community Plan. In order to demonstrate such conformity, the owners, applicants or developers, as applicable, must provide appropriate landscape, streetscape, on-site lighting, building and redevelopment activities and improvements to comport with the goals and intents of said Plan.

532 Purpose. The purpose of this 'Special Management District' is to revitalize and develop Tiogue Avenue/Nooseneck Hill Road and Sandy Bottom Road so as to dramatically improve the functioning and appearance of this arterial corridor, create redevelopment opportunities and create jobs as well as enhance the Town's economic tax base.

533 Applicability. This section shall apply to all real estate and buildings thereon situated on or adjacent to Tiogue Avenue and Nooseneck Hill Road and Sandy Bottom Road that are in a General Business (GB) Zoning District.

534 Procedure for approval. All new commercial uses, changes in commercial uses or expansion of commercial uses shall be subject to the provisions of this Article in addition to any other applicable Zoning Ordinance, Subdivision & Land Use Regulations. Such uses shall be reviewed by the Planning Department and/or Planning Commission as appropriate.

535 Architectural Standards.
 A. General. The architectural design of buildings, structures and site layout shall be visually compatible with the traditional historic character of Coventry and the surrounding area, including building materials, scale and roof line.

B. Specific. New buildings shall be designed with traditional roof forms that are compatible with other traditional New England towns such as gabled, lipped, and pitched roofs. Flat Roofs shall be avoided.

Architectural elements to include dormers, columns, facades, awnings, canopies, etc. shall be in proportion with the dimensions and design of the overall building. Exaggerated or excessively large architectural elements shall be avoided.

Traditional building materials such as shingles, wood clapboards, stucco, brick and stone should be used for the exterior sheathing of additions and new construction.

The construction of buildings which are designed primarily according to themes or architectural styles associated with chain stores or restaurants shall be reviewed by the Planning Commission and may be modified or prohibited if found inconsistent with the Town's Comprehensive Community Plan or this Ordinance.

For new retail or commercial establishments which will exceed 10,000 gross square feet of floor area or additions which will increase their floor area in

excess of 50% the following minimum standards shall apply:

1. There shall be no blank, windowless uninterrupted façade in excess of 60 feet in length. Lengthy facades shall be interrupted by recesses, projections, windows, awnings or similar measures.
2. Smaller retail stores that are a portion of a larger store or principal building shall have display windows and separate outside entrances.
3. Roof lines shall be varied so as to break up lengthy, monotonous facades and exteriors.

536 Colors and Materials. To ensure higher quality development, new or renovated buildings in the special management district shall meet the following 'colors and materials' standards:

1. Exterior materials shall be of high quality such as wood, brick, stucco, sandstone, or nature stone;
2. Façade colors shall be low reflective, subtle, neutral or earth-tone colors;
3. High-intensity or metallic colors on the exterior of buildings are prohibited;
4. Neon tubing as an accent material is prohibited.

537 Relationship to Surrounding Community. To ensure that all buildings with 10,000 square feet or more of gross floor area are compatible with surrounding streets, commercial and residential uses all new buildings in excess of 10,000 square feet gross floor area or existing buildings which propose to increase their gross floor area in excess of 50% shall:

1. Install or provide all façades that are visible from adjoining properties and/or public streets which encourage community integration by featuring characteristics of the front façade;
2. All sides of a principal building which directly faces abutting streets shall include at least one (1) customer entrance;
3. Evergreen trees must be installed on all sides of a building which abuts or faces a residential zoning district;
4. Landscape features shall be incorporated into all plans which shield loading docks, trash collection, and outdoor storage from public view;
5. Every effort shall be made to incorporate community and public spaces such as water fountains, water features, clock towers, and/or patio seating areas in plans.

538 General Business Design Guidelines. Those techniques and measures set forth in the 'Town of Coventry, Special Management District, General Business Guidelines' prepared by the Town of Coventry Planning Department and dated July, 2007 are incorporated herein (*street lighting should be coordinated through the local fire district).



7.0 References



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- City of Arlington Development Design Guidelines – <http://wa-arlington.civicplus.com/documents/Planning%20Division/devdesignguide.pdf>
- City of Hopkins Minnesota Downtown Design Guidelines – <http://www.hopkinsmn.com/planning/design.html>
- Wikipedia – www.wikipedia.org
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- GrowSmart RI – www.growsmartri.com
- U.S. Green Building Council – www.usgbc.org
- 10,000 Rain Gardens – <http://www.rainkc.com/home/index.asp>